



Commander (oan-2)  
Eleventh Coast Guard District

Bldg. 50-6, Coast Guard Island  
Alameda, CA 94501-5100  
Staff Symbol: (oan-2)  
Phone: (510) 437-3514  
FAX: (510) 437-5836

16590  
San Francisco Bay (8.9)  
Ser: 540-01  
December 19, 2001

Mr. H. P. Hensley  
California Department of Transportation  
P. O. Box 23660  
Oakland, CA 94623-0660

Dear Mr. Hensley:

Your application dated May 16, 2001 for Coast Guard approval of the location and plans of a proposed bridge replacement across San Francisco Bay, mile 8.9, between Oakland and Yerba Buena Island has been approved by the Commandant, U. S. Coast Guard. The instrument of approval, Bridge Permit (3-01-11), dated December 11, 2001, is provided as enclosure (1). Strict compliance with all terms of the permit is required.

Please review the conditions of the permit, particularly the time restrictions on commencement and completion. If you are unable to meet these time limits, you must request an extension of time at least 30 days in advance of the expiration date of your permit. This request can be made to our office at the address provided above.

This office must be kept informed concerning the status of the project. Please advise us of any pre-construction conferences, and furnish the date of construction at least 30 days prior to commencement to allow proper review and approval of the proposed method of construction. Progress reports will be established, giving a brief description of scheduled work, and showing the percentage of the project completed. The Coast Guard may conduct periodic inspections to ensure the work is not unreasonably interfering with navigation.

Upon completion of the project, submit written certification that the proposed structure has been built in accordance with the approved plans and that all conditions of the permit have been fulfilled. A bridge completion report form is provided as enclosure (2). When available, please provide 8 x 10-inch photographs, showing the completed structure as it would be seen by the mariner from both upstream and downstream.

Enclosure (3) is the approved navigational lighting and vertical clearance gauge plan, with text and drawings, for the proposed bridge. The lighting plan is prescribed in accordance with Title 33, Code of Federal Regulations, Part 118 (33 CFR 118). The prescribed navigational lighting is required to be installed and maintained on the portions of the bridge as they are constructed, to ensure the safety of navigation. Please note there are two plans, one prior to removal of the existing San Francisco-Oakland Bay Bridge East Span, and one following the removal.

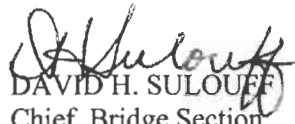
16590

December 19, 2001

Condition 5 of the permit discusses clearance gauges. Guidance under 33 CFR 118.160 is available on the internet at <http://www.access.gpo.gov/nara/cfr/index.html>. We have determined the visibility/safety distance requirement for the largest vessels approaching this bridge will be greater than 2,000 feet. Therefore the 36-inch high, Series "E" numerals with 10-ft. spacing are required. Please submit drawings of the proposed gauge installation for our review and approval, prior to installation.

You may contact Mr. Jerry Olmes, Project Manager, at (510) 437-3515 to acknowledge receipt of this permit.

Sincerely,



DAVID H. SULOUFF  
Chief, Bridge Section  
Eleventh Coast Guard District  
By direction of the District Commander

Encl: (1) Permit  
(2) Completion Report Form  
(3) Prescribed navigational lighting and clearance gauges

Copy: CG Marine Safety Office San Francisco Bay (w/lighting plan)  
CG Group San Francisco (w/lighting plan)  
Federal Highway Administration, Sacramento (w/o encl.)



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## BRIDGE PERMIT

(3-01-11)

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DEC 11 2001

**WHEREAS** by Title V of an act of Congress approved August 2, 1946, entitled "General Bridge Act of 1946," as amended (33 U.S.C. 525-533), the consent of Congress was granted for the construction, maintenance and operation of bridges and approaches thereto over the navigable waters of the United States;

**AND WHEREAS** the Secretary of Transportation has delegated the authority of Section 502(b) of that act to the Commandant, U. S. Coast Guard, by Section 1.46(c) of Title 49, Code of Federal Regulations;

**AND WHEREAS** before construction is commenced, the Commandant must approve the location and plans of any such bridge and may impose any specific conditions relating to the construction, maintenance and operation of the structure deemed necessary in the interest of public navigation, such conditions to have the force of law;

**AND WHEREAS** the - STATE OF CALIFORNIA - has submitted for approval the location and plans of a bridge to be constructed across the San Francisco Bay between Oakland and San Francisco, California,

**NOW THEREFORE**, This is to certify that the location and plan sheet 1 (of 3) dated June 2001, sheet 2 dated Jan 2001 and Sheet 3 revised 7 June 2001 are hereby approved by the Commandant, subject to the following conditions:

1. No deviation from the approved plans may be made either before or after completion of the structure unless the modification of said plans has previously been submitted to and received the approval of the Commandant.
2. The construction of falsework, cofferdams or other obstructions, if required, shall be in accordance with plans submitted to and approved by the Commander, Eleventh Coast Guard District, prior to construction of the bridge. All work shall be so conducted that the free navigation of the waterway is not unreasonably interfered with and the present navigable depths are not impaired. Timely notice of any and all events that may affect navigation shall be given to the District Commander during construction of the bridge. The channel or channels through the structure shall be promptly cleared of all obstructions placed therein or caused by the construction of the bridge to the satisfaction of the District Commander, when in the judgment of the District Commander the construction work has reached a point where such action should be taken, but in no case later than 180 days after the bridge has been opened to traffic.

**ENCLOSURE(1)**



3. Issuance of this permit does not relieve the permittee of the obligation or responsibility for compliance with the provisions of any other law or regulation as may be under the jurisdiction of the U. S. Army Corps of Engineers, San Francisco District; U. S. Department of the Interior, Fish And Wildlife Service; U. S. Department of Commerce, National Marine Fisheries Service; State of California: State Historic Preservation Officer; San Francisco Bay Conservation and Development Commission, or any other federal, state or local authority having cognizance of any aspect of the location, construction or maintenance of said bridge.

4. The location of, and materials to be used in construction of, the pier protection fender system as shown on the approved plan sheet 3 (of 3) revised 7 June 2001 shall be submitted to the District Commander for approval prior to commencing construction of such system.

5. Clearance gauges shall be installed and maintained in a good and legible condition by and at the expense of the owner of the bridge when so required by the District Commander. The type of gauges and the locations in which they are to be installed will be submitted to the District Commander for approval.

6. The permittee shall coordinate the methods and schedule of construction of this bridge with the Commander, Eleventh Coast Guard District, for the purpose of keeping project impacts on U. S. Coast Guard operations and facilities to a minimum. Coast Guard operational, logistical support and facility needs, including but not limited to delivery of fuel and other supplies, vehicular and pedestrian access, shuttle transportation services, construction of access roads, relocation of existing buildings, and construction of new facilities, maintenance of utilities, maintenance of and stabilization of existing slopes, will be to the satisfaction of the District Commander. Any violations of this condition renders this bridge permit null and void.

7. All parts of the existing to-be-replaced San Francisco - Oakland Bay Bridge across the San Francisco Bay, mile 8.9, not utilized in the new bridge shall be removed, by and at the expense of the owner of the bridge, down to or below 1.5 feet below the mud line of the waterway measured at time of removal, and the waterway cleared to the satisfaction of the District Commander. A period of three years subsequent to the opening to traffic of the new bridge, mile 8.9, will be allowed for such removal and clearance. The proposed method and schedule for demolition and removal of the bridge shall be submitted to the District Commander for approval prior to commencing such removal.

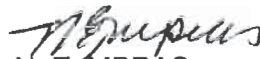
8. When the proposed bridge is no longer used for transportation purposes, it shall be removed in its entirety or to an elevation deemed appropriate by the District Commander and the waterway cleared to the satisfaction of the District Commander. Such removal and clearance shall be completed by and at the expense of the owner of the bridge upon due notice from the District Commander.

Continuation Sheet

**Bridge across the San Francisco Bay Between Oakland and  
San Francisco, California**

DEC 11 2001  
BRIDGE PERMIT  
(3-01-11)

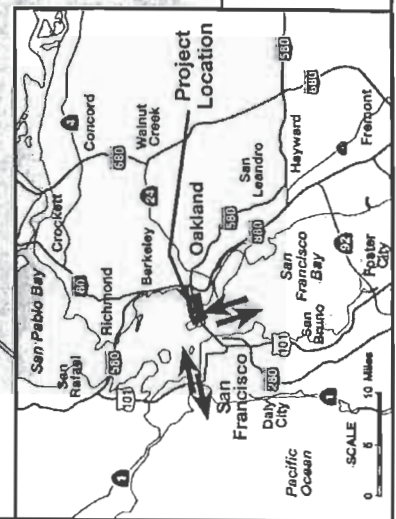
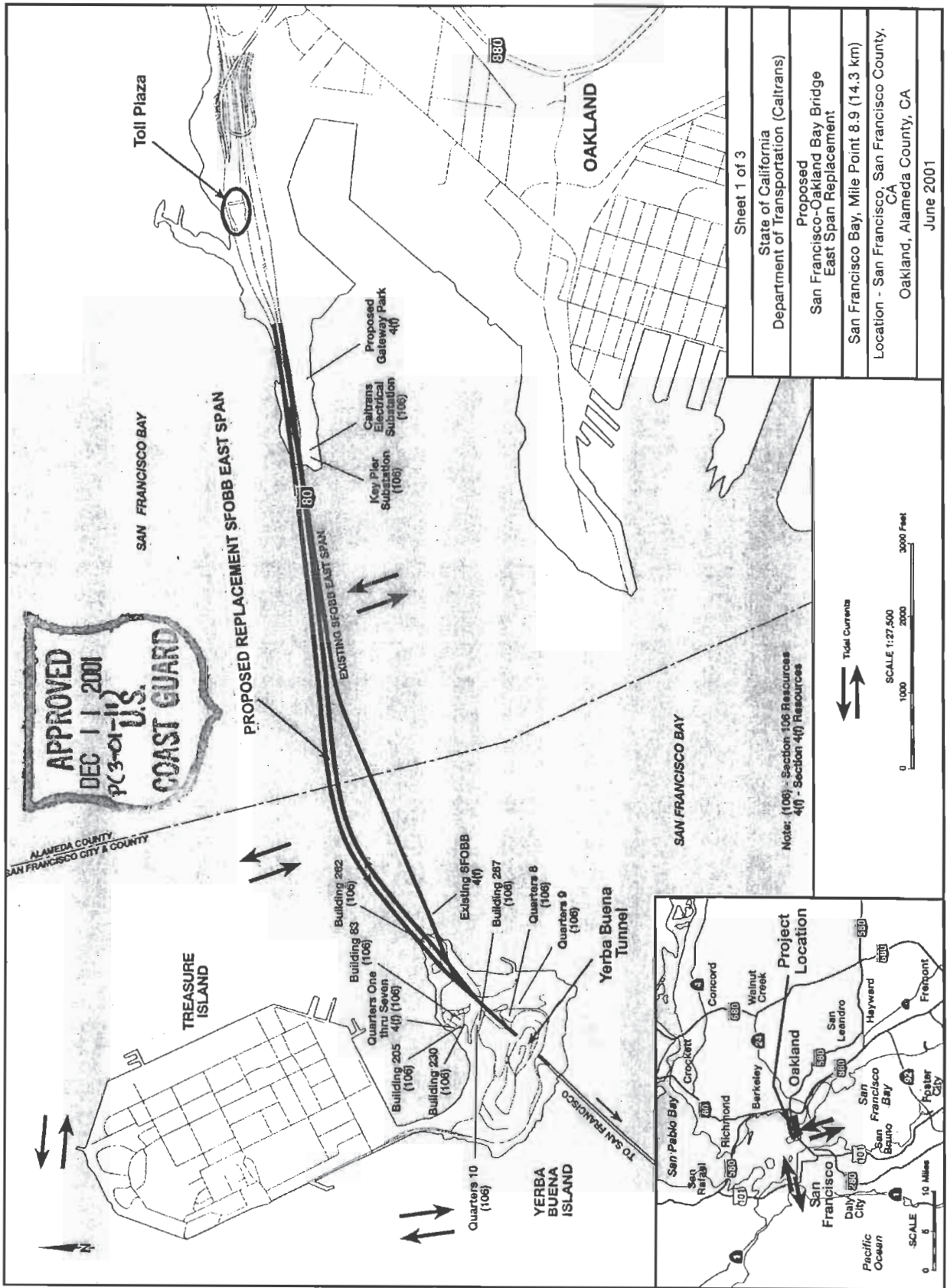
9. The approval hereby granted shall cease and be null and void unless construction of the bridge is commenced within three years and completed within eight years after the date of this permit.



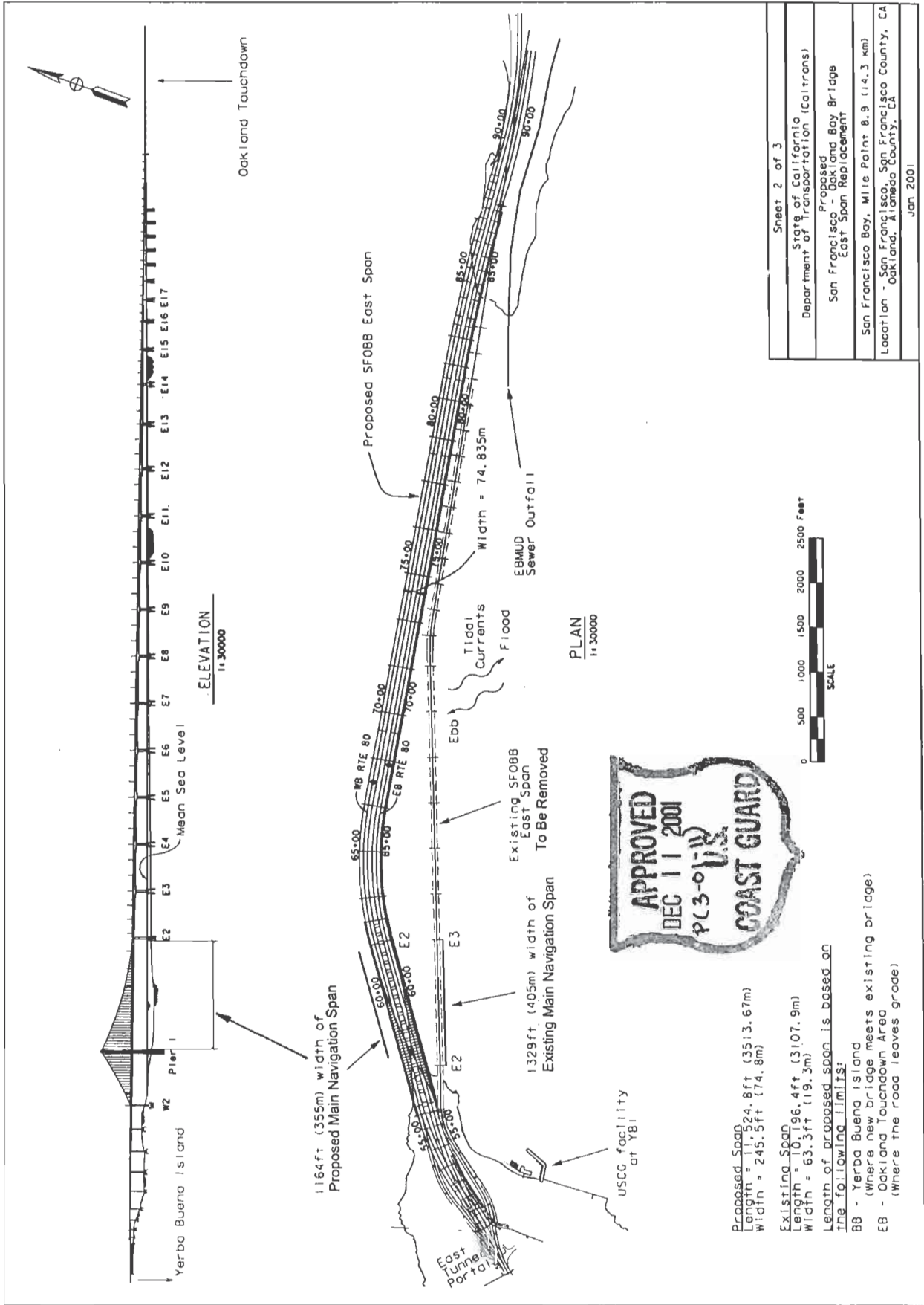
N. E. MPRAS  
Chief, Office of Bridge Administration  
U. S. Coast Guard  
By direction of the Commandant







Sheet 1 of 3
State of California Department of Transportation (Caltrans)
Proposed San Francisco-Oakland Bay Bridge East Span Replacement
San Francisco Bay, Mile Point 8.9 (14.3 km)
Location - San Francisco, San Francisco County, CA Oakland, Alameda County, CA
June 2001



Sheet 2 of 3

State of California  
 Department of Transportation (Caltrans)

Proposed  
 San Francisco - Oakland Bay Bridge  
 East Span Replacement

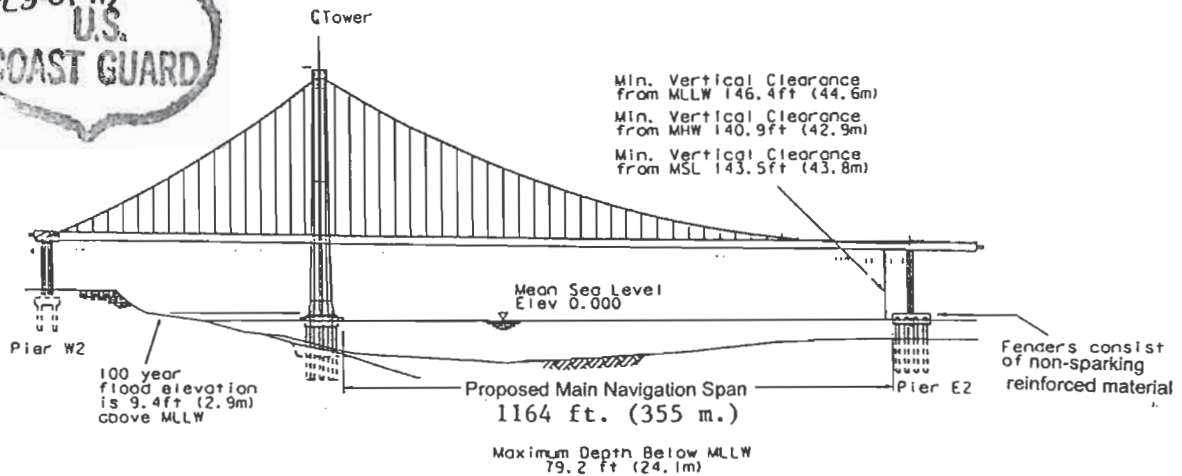
San Francisco Bay, Mile Point B.9 (14.3 km)

Location - San Francisco, San Francisco County, CA  
 Oakland, Alameda County, CA

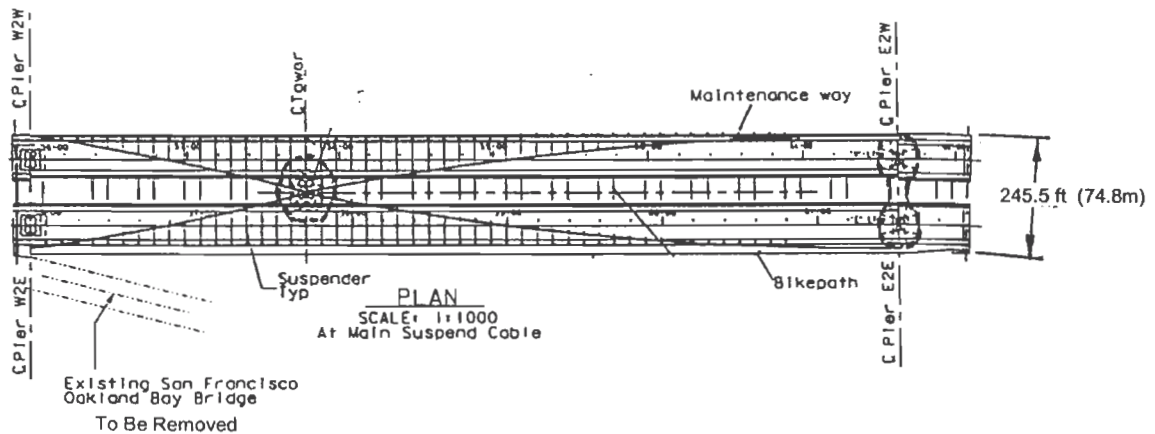
Jan 2001



# NAVIGATION OPENING



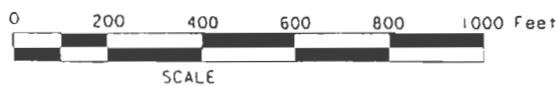
ELEVATION  
SCALE: 1:1000



Ebb  
 Tidal Currents  
 Flood

Note:  
 Datum is NGVD 29 = 0.00ft (0.00m)

Revised by U. S. Coast Guard  
 June 7, 2001



Sheet 3 of 3
State of California Department of Transportation (Caltrans)
Proposed San Francisco - Oakland Bay Bridge East Span Replacement
San Francisco Bay, Mile Point 8.9 (14.3 km)
Location - San Francisco, San Francisco County, CA Oakland, Alameda County, CA
Jan 2001



## **RECORD OF DECISION**

### **PROPOSED SAN FRANCISCO - OAKLAND BAY BRIDGE REPLACEMENT EAST OF YERBA BUENA ISLAND ACROSS THE SAN FRANCISCO BAY, MILE 8.9, BETWEEN SAN FRANCISCO AND OAKLAND, CALIFORNIA P(3-01-11)**

#### **I. DESCRIPTION OF THE PROPOSED PROJECT**

The State of California Department of Transportation (CALTRANS) proposes to construct a replacement high-level, fixed highway bridge, east of Yerba Buena Island (YBI), across the San Francisco Bay, mile 8.9, on U. S. Interstate Highway 80, connecting Oakland and San Francisco. The 11,526-foot project extends from the east end of the YBI tunnel to west of the bridge toll plaza on the Oakland Touchdown. The purpose of this project is to provide a seismically protected structure which can withstand earthquake damage and be used as a lifeline transportation corridor for evacuation and emergency services following another major earthquake in the Bay area. The existing double-decked, five-lane, high-level fixed bridge is not capable of withstanding another major earthquake and will be removed.

The Federal Highway Administration (FHWA) is the lead federal agency for satisfying requirements of the National Environmental Policy Act (NEPA). A Draft Environmental Impact Statement/Statutory Exemption (DEIS/SE) for the proposed project was approved by the FHWA on September 21, 1998. A Supplemental Section 4(f) Draft Evaluation was prepared by the FHWA in June 1999, and the Final EIS/SE/Final Section 4(f) Evaluation was approved by the FHWA on April 17, 2001. The Final EIS/SE/Final Section 4(f) Evaluation was filed with the EPA on May 18, 2001. No changes have been made since the Final EIS/SE/Final Section 4(f) Evaluation was approved by FHWA.

The Coast Guard was a cooperating agency in preparing the environmental document. Action by the Coast Guard consists of issuance or denial of a bridge permit for the proposed bridge project. Coast Guard NEPA responsibility is to assess the navigational and environmental impacts of construction, maintenance and operation of the proposed fixed bridge.

#### **II. DECISION**

The Commander, Eleventh Coast Guard District, has recommended, and the Commandant, U. S. Coast Guard, has decided to approve the location and plans for the proposed bridge project. This decision is considered to be in the best public interest for satisfying project objectives with the least impacts on navigation and on the environment.

#### **III. ALTERNATIVES CONSIDERED**

Alternatives initially considered were four alignments north of the existing structure, and three alignments south of the existing structure. There were several design alternatives within these

basic alignment alternatives. Traffic projections were not included because traffic flows will continue to be limited by the tunnel on YBI. These alternatives were rejected for a variety of reasons, including adverse geologic conditions, inability to meet AASHTO design standards, less than optimal roadway geometry, constructability issues for temporary detours, conflicts with the East Bay Municipal Utilities District sewer outfall, and limitations on panoramic views. Subsequently, five additional alternatives were considered: no-build; a high-level, fixed bridge on three different alignments (S-4), (N-2) and (N-6) with the same touchdown points; and the Retrofit Existing Structure Alternative. The no-build alternative would have the least adverse impact on the environment.

The preferred Replacement Alternative N-6 would avoid land from the Oakland Army Base on the Oakland Touchdown to be reutilized for the proposed Gateway Park, and meets the established needs of the project. The U. S. Environmental Protection Agency and U. S. Army Corps of Engineers concur Alternative N-6 is the Least Environmentally Damaging Practical Alternative (LEDPA), required by Section 404 of the Clean Water Act.

An expanded description of the various alternatives and recommended (preferred) alternative, including the basis for the decision, is included in the summary of the FEIS/SE/Final Section 4(f) Evaluation. After considering responses to the Coast Guard public notices, the impacts associated with each alternative and the present and future transportation needs, I have determined that the proposed project's impacts of the selected (preferred) alternative cannot be avoided, and all planning and mitigation to minimize these impacts have been accomplished.

#### **PREFERRED ALTERNATIVE**

The proposed bridge would result in the construction of two parallel, five-lane, high-level, fixed highway decks on a single set of piers. The proposed bridge would have sufficient clearance to ensure no adverse impact on the navigable waterway.

The preferred alternative, proposed design, for a self-anchored, suspension bridge, would provide the following clearances in the navigation span:

Horizontal as measured between fenders, normal to axis of channel	1,164 ft. (355 m)
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Minimum vertical clearance above MHW Elev. 2.6' (0.8 m) [1929 NGVD]	140.9 ft. (42.9 m)
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Minimum vertical clearance above MLLW Elev. -2.9' (-0.8m) [1929 NGVD]	146.4 ft. (44.6m)
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#### **IV. BASIS FOR DECISION**

After an independent review of the FHWA FEIS/SE/Final Section 4(f) Evaluation, approved April 17, 2001, by the FHWA, I have determined that the environmental document adequately identifies and assesses the impacts of the proposed bridge project across the San Francisco Bay, mile 8.9, east of YBI, between Oakland and San Francisco, California, including the building

relocations. The Commander, Eleventh Coast Guard District, adopted the FEIS/SE/Final Section 4(f) Evaluation on December 4, 2001.

The FEIS/SE/Final Section 4(f) Evaluation contains an adequate detailed statement of the following: project description and purpose, probable impacts of the project, alternatives, unavoidable adverse environmental effects, short-term impacts versus long-term benefits, irreversible and irretrievable commitment of resources and measures to minimize environmental harm. The proposal conforms with the State of California's air quality implementation plan and with the national ambient air quality standards.

## **V. MITIGATION**

The San Francisco Bay provides habitat for anadromous salmonids, the endangered California least tern and California brown pelican. Eelgrass beds, which provide wetland-spawning habitat for the Pacific herring, a commercial fish species, are located within the project area and are protected under the Magnuson-Stevens Fishery Conservation and Management Act. Sand flats providing roosting and feeding habitat for the common loon, long billed curlew and elegant tern, as migratory birds protected under the Migratory Bird Treaty Act, are also located within the project area. Section 7 consultations will ensure mitigation to reduce impacts to these species and habitats. Wetlands mitigation would include on-site eelgrass bed and sand flat restoration, funding for off-site wetlands mitigation, marsh restoration plans, and monitoring. The National Marine Fisheries Service and Fish and Wildlife Service Biological Opinions endorse proposed on-site mitigation including bubble curtains to reduce fish mortality, and off-site measures including upland disposal of potentially contaminated sediments to improve long-term species benefits. All reasonable efforts will be made to minimize negative impacts to fish and wildlife resources.

A Memorandum of Agreement (MOA), executed by the Advisory Council on Historic Properties, addresses mitigation efforts to historic properties, including salvage, permanent interpretive exhibits and display of exhibit materials, a museum exhibit, oral history, and documentary videos. It also addresses development of a comprehensive treatment plan for archaeological resources.

Mitigation efforts to minimize harm to 4(f) properties would include monitoring of historic structures during construction, restoration of historic grounds after construction, restoration of any inadvertent damage to historic structures to the Secretary of the Interior's standards, and the various mitigation efforts to historic properties outlined in the MOA.

To address the adverse effects of the project upon archaeological site CA-SFR-04, Caltrans prepared a draft report entitled Research Design and Treatment Plan for Archaeological Site CA-SFR-04/H, Yerba Buena Island, San Francisco, California, dated October 2001, pursuant to Section 106 of the National Historic Preservation Act, 16 U.S.C. §470f. Because a portion of Site CA-SFR-04/H (midden deposit) is located on Coast Guard property, the Coast Guard intends to issue a permit for excavation and removal of the midden deposit, pursuant to the Archaeological Resources Protection Act, 16 U.S.C. §470cc. (ARPA).



The proposed alignment would involve relocation of Building 213 (fire engine storage) for construction of the temporary detours. The project would also involve removal and replacement of three vacant buildings and one storage building. There would be no socioeconomic impacts to these properties. Relocation assistance would be consonant with the Uniform Relocation Assistance and Real Property Acquisition Act of 1970 (Public Law 91-646). There would be no displacement or relocations of residents, and the project will not affect minority or low-income populations under Environmental Justice requirements.


The proposed project will involve potential impacts to the U. S. Coast Guard Group San Francisco facilities and operations. The permittee will consult with the Coast Guard to develop feasible and reasonable measures to reduce construction related noise levels in the vicinity of Coast Guard facilities. Coast Guard properties will be monitored for construction related damage including the use of vibration measuring devices on buildings. Additional mitigation includes shuttle transit during bus stop staircase displacement, 24/7 emergency access during access road realignment, and slope restoration and restabilization following construction. On-going negotiations are underway between appropriate Coast Guard commands and the California Department of Transportation to address specific methods regarding the mitigation recommendations.

Minimization, avoidance, or elimination of adverse impacts was a primary consideration throughout the project planning. All efforts have been made to minimize impacts on the environment and on navigation.

## **VI. CONCLUSION**

Based upon an independent Coast Guard review of all pertinent factors, including navigation and the human environment, I conclude that the proposed replacement of the San Francisco - Oakland Bay Bridge, east of Yerba Buena Island, will meet the reasonable needs of navigation with no unmitigated, significant adverse impacts on the quality of the human environment.

Date: DEC 11 2001

  
\_\_\_\_\_  
N. E. MPRAS  
Chief, Office of Bridge Administration  
U. S. Coast Guard  
By direction of the Commandant

COAST GUARD COMPLETION REPORT

16591  
San Francisco Bay (8.9)

From: \_\_\_\_\_  
To: Commander, 11th Coast Guard District  
Bldg 50-6  
Coast Guard Island  
Alameda, CA 94501-5100

DATE: \_\_\_\_\_

Our bridge project is completed and the following information is submitted for your records as requested by your letter dated \_\_\_\_\_;

1. Construction of the substructure started on \_\_\_\_\_,  
and substructure construction was finished on \_\_\_\_\_.
2. Construction of the superstructure started on \_\_\_\_\_,  
and superstructure construction was finished on \_\_\_\_\_.
3. The new bridge section was opened for use on \_\_\_\_\_.
4. The bridge provides \_\_\_\_\_ feet horizontal waterway clearance in \_\_\_\_\_  
navigable channel(s).
5. The bridge provides \_\_\_\_\_ feet vertical clearance above Mean Lower Low Water and  
the bridge also provides \_\_\_\_\_ feet vertical clearance above Mean High Water.
6. All parts of the former bridge at this location not incorporated in the new bridge have been removed to  
the mud line.
7. The bridge is owned by: the bridge is operated/maintained by:

_____	_____
_____	_____
_____	_____
tel: _____	tel: _____

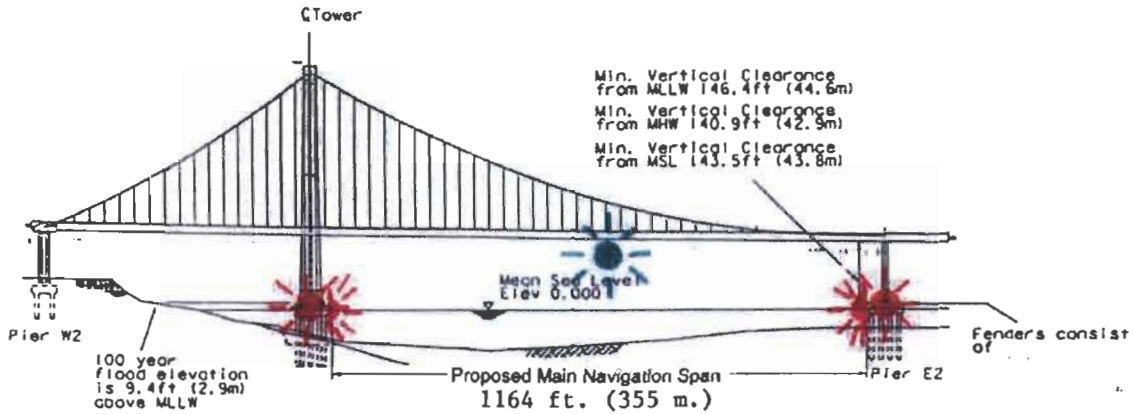
8. I certify that the bridge has been built in accordance with the approved permit plans (or USCG  
authorized modifications to such plans) and all requested modifications have been fulfilled. Photographs  
of the bridge are enclosed.

\_\_\_\_\_  
Signature

\_\_\_\_\_  
Printed name and title

ENCLOSURE(2)

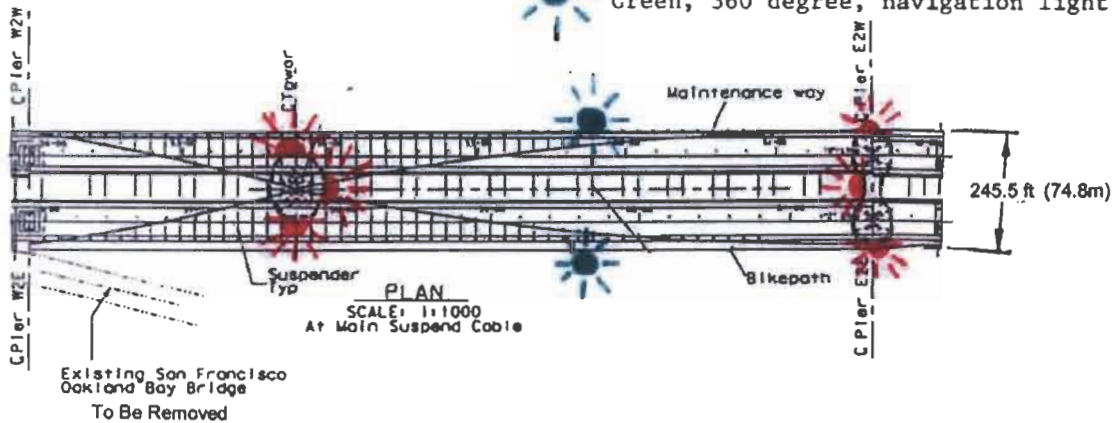
# NAVIGATION OPENING



Red, 180 degree, navigation light



Green, 360 degree, navigation light



Navigation Lights and/or Other Signals, approved as shown, pursuant to 33 CFR 118.

Date: 12/19/01

Signature: *David H. Sulouff*  
 DAVID H. SULOUFF  
 Chief, Bridge Section  
 11th Coast Guard District  
 By direction of District Commander

Note:  
 Datum is NGVD 29 + 0.00ft (0.00m)

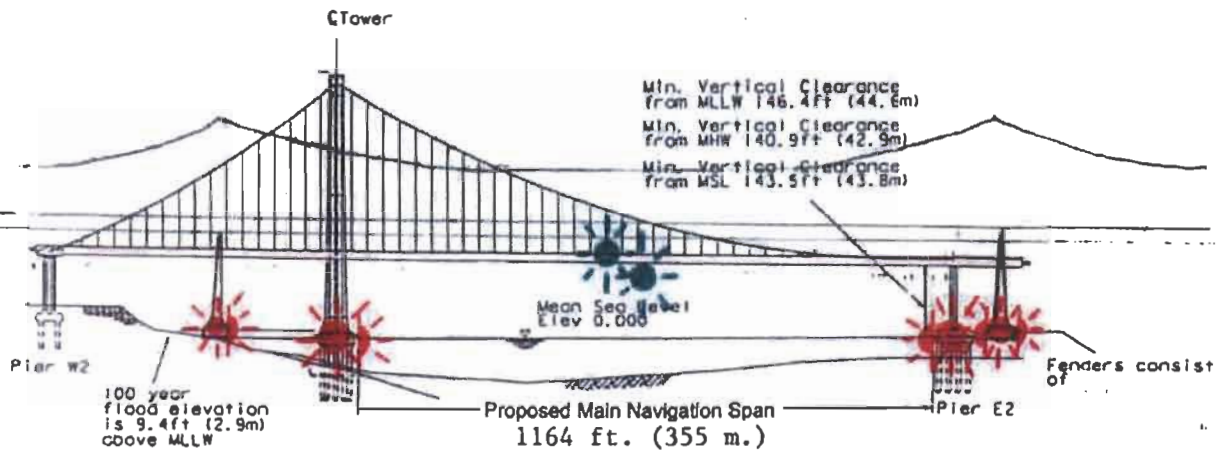


Proposed San Francisco - Oakland Bay Bridge East Span Replacement
San Francisco Bay, Mile Point 8.9 (14.3 km)
Location - San Francisco, San Francisco County, CA Oakland, Alameda County, CA
Jan 2001

ENCLOSURE(3)



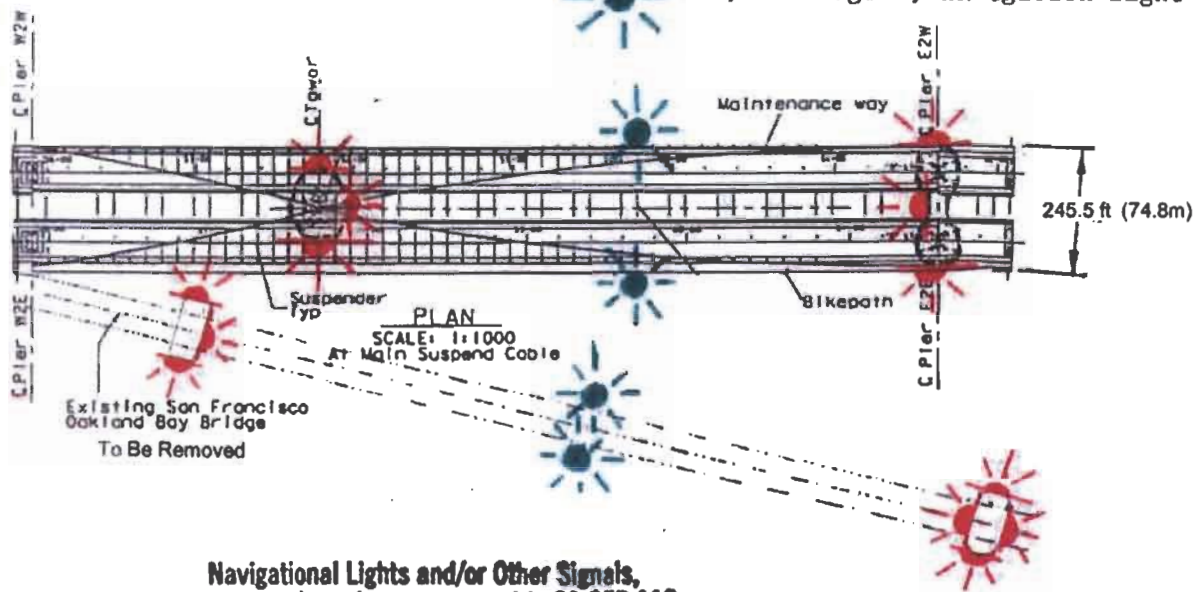
# NAVIGATION OPENING



Red, 180 degree, navigation light



Green, 360 degree, navigation light



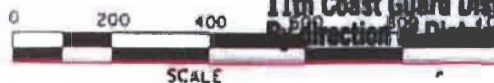
Navigational Lights and/or Other Signals, approved as shown, pursuant to 33 CFR 118.

Note:  
Datum is NGVD

Date: 12/19/01

Signature: *David H. Soldoff*

DAVID H. SOLDOFF  
 Chief, Bridge Section  
 11th Coast Guard District  
 Director of District Commander



Proposed  
 San Francisco - Oakland Bay Bridge  
 East Span Replacement

San Francisco Bay, Mile Point 8.9 (14.3 km)

Location - San Francisco, San Francisco County, CA  
 Oakland, Alameda County, CA

Jan 2001

Navigation Lighting Plan  
San Francisco – Oakland Bay Bridge  
San Francisco Bay, Mile 8.9  
Between Yerba Buena Island and Oakland

The center of the main navigation channel span of the bridge shall be marked by a range of two green lights. Each green light shall show through a horizontal arc of 360 degrees and shall be securely mounted just below the outermost edges of the bridge span structure, so as to be visible from an approaching vessel.

The upstream and downstream ends of the left and right main navigation channel pier protection system shall be lighted with red lights. Each light shall show through a horizontal arc of 180 degrees, and shall be securely fastened at the end of the protection structure as low as practicable but not lower than 2 feet above navigable high water, to show 90 degrees on either side of a line parallel to the axis of the channel, so as to be visible from an approaching vessel.

The channelward faces of the left and right main navigation channel pier protection system shall be lighted with single red lights. Each light shall show through a horizontal arc of 180 degrees, and shall be securely fastened to the top of the protection structure, centered under the main navigation opening, to show 90 degrees on either side of a line perpendicular to the axis of the channel, so as to be visible from an approaching vessel.

The prescribed navigational lighting must be securely attached to the structure and of sufficient candlepower as to be visible against the background lighting at a distance of at least 2,000 yards, 90 percent of the nights of the year.

Vertical clearance gauges shall be installed on the north end of the west channel pier protection and on the south end of the east channel pier protection, so as to be visible by approaching vessels, and to show the clearance available for navigation between the water surface and the lowest hittable part of the channel span, measured to the bottom of the foot marks. The numerals for the gauges shall be black in color, on a white background. The numerals shall measure 36 inches in height and be spaced every 10 feet. The bottom of the bottom most numerals of both gauges shall indicate the least clear height measured at the east channel pier, and shall be located 1.5 feet below mean sea level (NGVD 29) (MSL). The bottom of the topmost numerals of both gauges shall show the least clear height measured at the east channel pier, and shall be located 8.5 feet above MSL.

The bridge owner is responsible for initial installation and continued maintenance of each navigation aid in good repair and legible condition.

U.S. Department of  
Homeland Security

United States  
Coast Guard



Commander  
Eleventh District

U.S. Coast Guard Island,  
Building 50-2  
Alameda, CA 94501-5100  
Staff Symbol: (dpw)  
Phone: (510) 437-3514  
Fax: (510) 437-5836

16591  
San Francisco Bay (8.9)  
November 28, 2011

California Department of Transportation  
Attn: Steven Hulsebus  
P. O. Box 23660  
Oakland, CA 94623-0660

Dear Mr. Hulsebus:

Your application of October 20, 2008, revised January 12, 2011, for Coast Guard approval for an extension of time to complete the replacement eastern span of the San Francisco/Oakland Bay Bridge, mile 8.9, San Francisco Bay, between the cities of Oakland, Alameda County and San Francisco, San Francisco County, California has been approved by the Commandant, U.S. Coast Guard. The instrument of approval, Bridge Permit Amendment (3a-01-11), dated November 18, 2011, is enclosed. Strict compliance with all terms of the permit is required.

Please review the time conditions of the permit. If you are unable to meet the time limit, you must request an extension of time at least 30 days in advance of the expiration of your permit. This request can be made to our office at the address listed above.

This office must be kept informed concerning the status of the project. Progress reports will be required, giving a brief description of the work accomplished. The Coast Guard will conduct periodic inspections to ensure that the work is not unreasonably interfering with navigation.

Upon completion of the entire project, submit written confirmation that the new structure has been built in accordance with the approved plans, and that all conditions of the permit have been fulfilled. A bridge completion report form is enclosed. When available, please provide 8" X 10" glossy black and white photographs, showing the completed structure, as would be seen by the mariner, from both upstream and downstream.

You may contact Mr. Carl Hausner, Project Manager, by telephone at (510) 437-3515 to provide updates or to discuss this project.

Sincerely,

A handwritten signature in black ink, appearing to read "D. Sulouff", written over a horizontal line.

David M. Sulouff  
Chief, Bridge Section  
Eleventh Coast Guard District  
By direction of the District Commander

Encl 1 P(3a-01-11)  
Encl 2 Project Completion Report





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## BRIDGE PERMIT AMENDMENT (3a-01-11)

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18 NOV 2011

**WHEREAS** by a permit issued on 11 December 2001, the Commandant of the Coast Guard approved the location and plans of a bridge to be constructed by the State of California across the San Francisco Bay between Oakland and San Francisco, California, under authority of the General Bridge Act of 1946, as amended;

**AND WHEREAS** condition 7 of that permit requires that the existing, to-be-replaced, bridge be removed no later than three years subsequent to the opening to traffic of the new bridge, and condition 9 fixed the time for completing construction of the bridge at 11 December 2009 and the - STATE OF CALIFORNIA - now requests that the time for removal of the existing, to-be-replaced, bridge be extended and requests that the time for completing construction of the bridge be extended;

**NOW THEREFORE**, This is to certify that said requests are hereby approved. In granting this approval, all conditions to which the original permit was subject remain in force with conditions 7 and 9 modified as follows:

7. All parts of the existing to-be-replaced San Francisco - Oakland Bay Bridge across the San Francisco Bay, mile 8.9, not utilized in the new span shall be removed, by and at the expense of the owner of the bridge, down to or below 1.5 feet below the mud line of the waterway measured at time of removal, and the waterway cleared to the satisfaction of the District Commander. A period of six years subsequent of the opening to traffic of the new bridge, mile 8.9, will be allowed for such removal and clearance. The propose method and schedule for demolition and removal of the bridge shall be submitted to the District Commander for approval prior to commencing such removal.

9. The approval hereby granted shall cease and be null and void unless construction of the bridge is completed by 31 December 2013.

A handwritten signature in blue ink, reading "Hala Elgaaly".

HALA ELGAALY, P.E.  
Administrator, Bridge Program  
U. S. Coast Guard  
By direction of the Commandant

### Completion Report Information

Upon completion of the bridge, complete and return this information to:

Commander (dpw)  
Eleventh Coast Guard District  
Coast Guard Island, BLDG 50-2  
Alameda, CA 94501-5100

Name & Location of Bridge: Replacement Eastern Span San Francisco/Oakland Bay Bridge, Mile 8.9, San Francisco Bay

Owner: California Department of Transportation  
Address: \_\_\_\_\_  
City, State, ZIP: \_\_\_\_\_

Type of Project: Replacement

Date commenced: \_\_\_\_\_, Date Completed: \_\_\_\_\_

Horizontal navigational clearance, pier face to pier face, measured normal to the axis of the channel, between Pier T-1(Tower) and Pier E-2: \_\_\_\_\_ feet \_\_\_\_\_ meters.

Vertical navigational clearance measured to the lowest hittable part of the bridge (which includes installed vortex wind generators), above Datum, Mean High Water at Pier T-1 Tower: \_\_\_\_\_ feet \_\_\_\_\_ meters, at pier E-2: \_\_\_\_\_ feet \_\_\_\_\_ meters, and center span of T-1 and E-2 \_\_\_\_\_ feet \_\_\_\_\_ meters.

Vertical navigational clearance measured to the lowest hittable part of the bridge (which includes maintenance travelers), above Datum, Mean High Water at Pier T-1 Tower: \_\_\_\_\_ feet \_\_\_\_\_ meters, at pier E-2: \_\_\_\_\_ feet \_\_\_\_\_ meters, and center span of T-1 and E-2 \_\_\_\_\_ feet \_\_\_\_\_ meters.

Vertical Clearance Gauges: Not required by the USCG.

Navigational Lighting: Required by the USCG.

Pier Protection/ Fendering System: Required by the USCG.

Photographs, 8 X 10 inch, glossy, black & white ( ) included, ( ) to be provided later.

Bridge to be replaced: Eastern Span San Francisco/Oakland Bay Bridge.

Date bridge open to transportation: \_\_\_\_\_

I certify the above bridge has been built in accordance with Coast Guard Bridge Permit Amendment No. 3a-01-11, and all conditions of the permit have been fulfilled.

Signature: \_\_\_\_\_, Date: \_\_\_\_\_

Title: \_\_\_\_\_